

# TOWN OF BARNSTABLE, MASSACHUSETTS

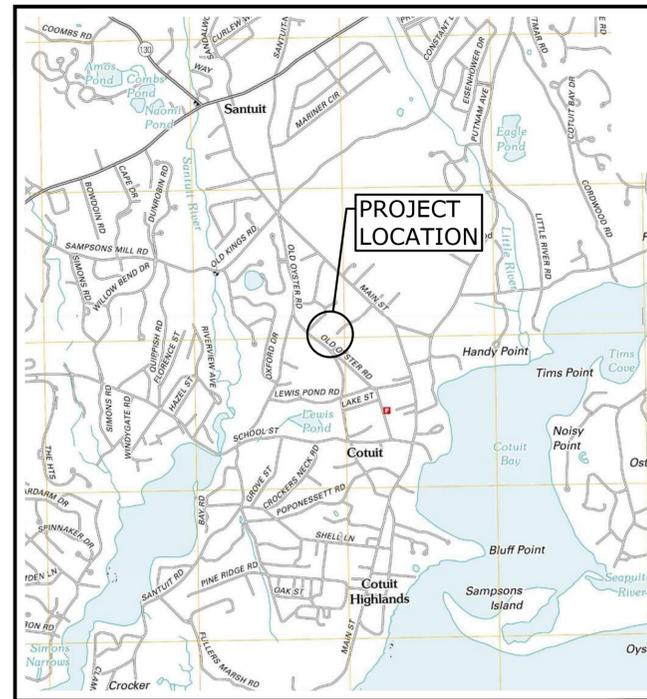
# COTUIT ELEMENTARY SCHOOL

## 140 OLD OYSTER ROAD

### DRAFT SET

### FEBRUARY 17, 2026

LIST OF DRAWINGS		
SHEET NO.	DRAWING NO.	DRAWING TITLE
GENERAL		
-	-	COVER SHEET
2	C-101	EXISTING CONDITIONS AND DEMOLITION PLAN
3	C-201	SITE AND GRADING PLAN
4	C-301	CIVIL DETAILS
5	L-101	LANDSCAPE PLANTING PLAN
6	L-201	LANDSCAPE DETAILS



**SITE LOCATION MAP**  
SCALE: 1" = 2000'

PREPARED BY:



177 Corporate Drive  
Portsmouth, NH 03801  
T 603.433.6818

PREPARED FOR:

**CLIENT**

BARNSTABLE FIRE DISTRICT  
PRUDENTIAL FIRE COMMITTEE  
255 BREEDS HILL ROAD  
HYANNIS, MA 02501

**SURVEYOR**

GREENMAN-PEDERSEN, INC.  
21 DANIEL ST., SECOND FLOOR  
PORTSMOUTH, NH 03801

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**COMPLETE SET 6 SHEETS**

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 Tighe & Bond: J:\B0633 Barnstable\031 Cotuit Elementary Feasibility Drawings\AutoCAD\Sheet\B0633-031\_C-COVER.dwg

**PLAN SET  
ISSUE TITLE**

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**Cotuit  
Elementary  
School**

Town of  
Barnstable

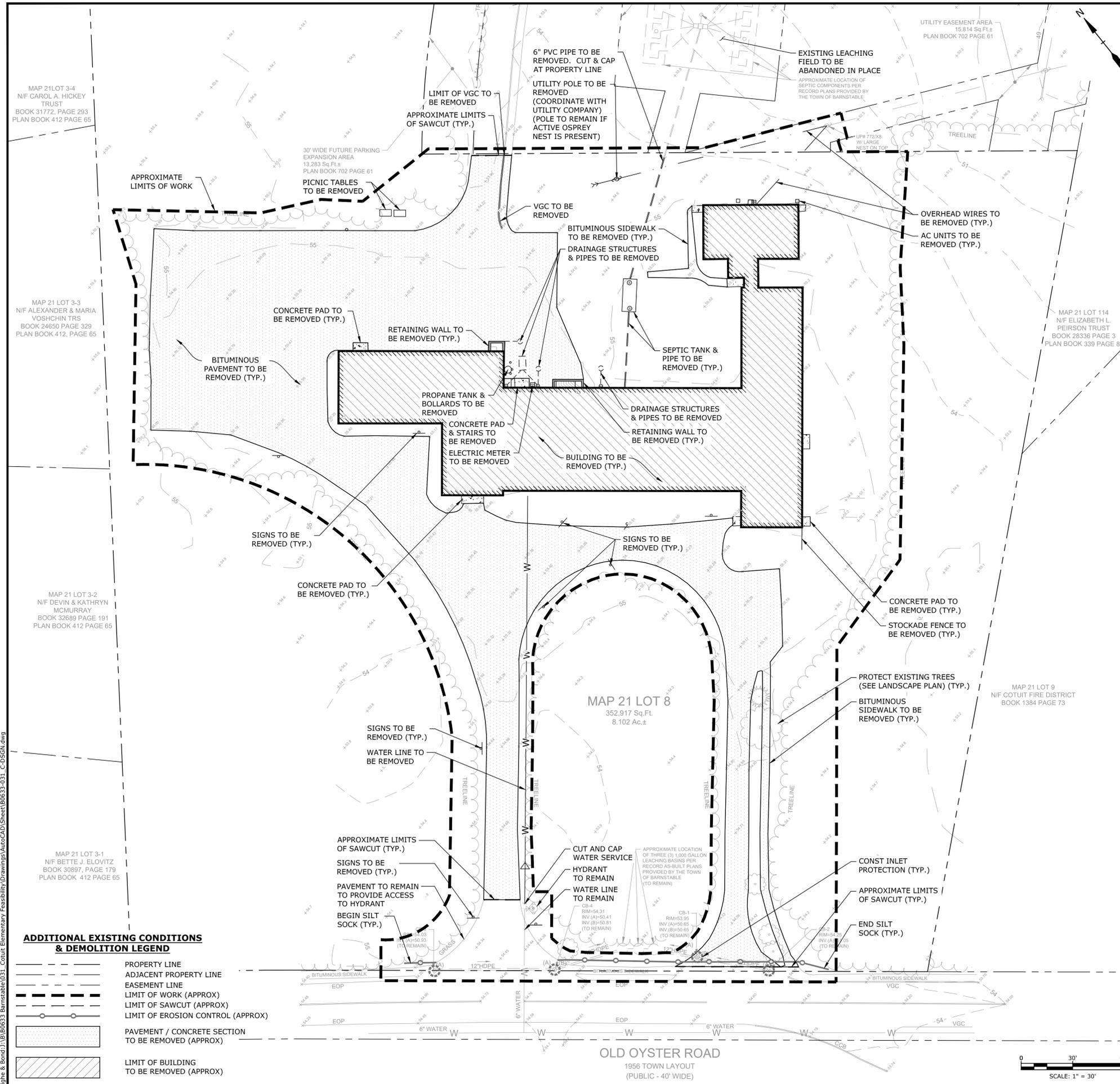
Barnstable,  
Massachusetts

MARK	DATE	DESCRIPTION
PROJECT NO:	B0633-031	
DATE:	02/17/2026	
FILE:	B0633-031_C-DSGN.dwg	
DRAWN BY:	M.CURLEY	
DESIGNED BY:	M.CURLEY	
CHECKED BY:	E.DOREMUS	
APPROVED BY:	P.CRIMMINS	

**EXISTING CONDITIONS  
AND DEMOLITION PLAN**

SCALE: AS SHOWN

- EXISTING CONDITIONS PLAN NOTES:**
- EXISTING CONDITIONS ARE BASED ON A FIELD SURVEY BY GREENMAN-PEDERSEN, INC., DATED 1/16/2026.
- DEMOLITION NOTES:**
- EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE START OF ANY CLEARING OR DEMOLITION ACTIVITIES.
  - IT IS THE CONTRACTOR'S RESPONSIBILITY TO FAMILIARIZE THEMSELVES WITH THE CONDITIONS OF ALL OF THE PERMIT APPROVALS.
  - THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE APPROXIMATE AND THE LOCATIONS ARE NOT GUARANTEED BY THE OWNER OR ENGINEER. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE ALL UTILITIES, ANTICIPATE CONFLICTS, REPAIR EXISTING UTILITIES, AND RELOCATE EXISTING UTILITIES REQUIRED TO COMPLETE THE WORK AT NO ADDITIONAL COST TO THE OWNER.
  - THE CONTRACTOR SHALL VERIFY LOCATION OF ALL EXISTING UTILITIES. CALL DIG SAFE AT LEAST 72 HOURS PRIOR TO THE COMMENCEMENT OF ANY DEMOLITION/CONSTRUCTION ACTIVITIES.
  - THE ACCURACY AND COMPLETENESS OF SUBSURFACE INFORMATION (E.G., EXISTING UTILITIES) SHOWN ON THESE DRAWINGS IS NOT GUARANTEED AND SOME SUBSURFACE INFORMATION MAY NOT BE SHOWN. DETERMINE THE LOCATIONS AND ELEVATIONS OF ALL SUBSURFACE FEATURES WHICH MAY AFFECT CONSTRUCTION OPERATIONS BY TEST PIT OR OTHER METHODS, AS NECESSARY TO PREVENT DAMAGE TO UTILITIES AND OTHER SUBSURFACE FEATURES, AND/OR INTERRUPTIONS IN UTILITY SERVICE. PROVIDE DATA COLLECTED THROUGH THESE INVESTIGATIONS TO THE ENGINEER PRIOR TO CONSTRUCTING THE PROPOSED IMPROVEMENTS.
  - THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY UTILITIES IDENTIFIED DURING CONSTRUCTION THAT ARE NOT SHOWN ON THE DRAWINGS OR THAT DIFFER IN SIZE OR MATERIAL.
  - ANY EXISTING WORK OR PROPERTY DAMAGED OR DISRUPTED BY CONSTRUCTION/DEMOLITION ACTIVITIES SHALL BE REPLACED OR REPAIRED TO MATCH ORIGINAL EXISTING CONDITIONS BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
  - THE CONTRACTOR IS RESPONSIBLE FOR SITE SAFETY; COORDINATION WITH THE OWNER, ALL SUBCONTRACTORS, AND WITH OTHER CONTRACTORS WORKING WITHIN THE LIMITS OF WORK, THE MEANS AND METHODS OF CONSTRUCTING THE PROPOSED WORK.
  - THE CONTRACTOR SHALL VERIFY ORIGIN OF ALL DRAINS AND UTILITIES PRIOR TO REMOVAL/TERMINATION TO DETERMINE IF DRAINS OR UTILITY ARE ACTIVE, AND SERVICES ANY ON OR OFF-SITE STRUCTURE TO REMAIN. THE CONTRACTOR SHALL NOTIFY ENGINEER IMMEDIATELY OF ANY SUCH UTILITY FOUND AND SHALL MAINTAIN THESE UTILITIES UNTIL PERMANENT SOLUTION IS IN PLACE.
  - ALL MATERIALS SCHEDULED TO BE REMOVED SHALL BECOME THE PROPERTY OF THE CONTRACTOR UNLESS OTHERWISE SPECIFIED. THE CONTRACTOR SHALL DISPOSE OF ALL MATERIALS OFF-SITE IN ACCORDANCE WITH ALL FEDERAL, STATE, AND LOCAL REGULATIONS, ORDINANCES AND CODES.
  - THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL EXISTING STRUCTURES, CONCRETE PADS, UTILITIES AND PAVEMENT WITHIN THE WORK LIMITS SHOWN UNLESS SPECIFICALLY IDENTIFIED TO REMAIN. ITEMS TO BE REMOVED INCLUDE BUT ARE NOT LIMITED TO: CONCRETE, PAVEMENT, CURBS, LIGHTING, MANHOLES, CATCH BASINS, UNDER GROUND PIPING, POLES, STAIRS, SIGNS, FENCES, RAMPS, WALLS, BOLLARDS, BUILDING SLABS, FOUNDATION, TREES AND LANDSCAPING.
  - REMOVE TREES AND BRUSH AS REQUIRED FOR COMPLETION OF WORK. CONTRACTOR SHALL GRUB AND REMOVE ALL STUMPS WITHIN LIMITS OF WORK AND DISPOSE OF OFF-SITE IN ACCORDANCE WITH FEDERAL, STATE, AND LOCAL LAWS AND REGULATIONS.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DEMOLITION AND OFF-SITE DISPOSAL OF MATERIALS REQUIRED TO COMPLETE THE WORK, EXCEPT FOR WORK NOTED TO BE COMPLETED BY OTHERS.
  - THE CONTRACTOR SHALL COORDINATE; REMOVAL, RELOCATION, DISPOSAL, ABANDONMENT, OR SALVAGE OF UTILITIES WITH THE OWNER AND APPROPRIATE UTILITY COMPANY.
  - UTILITIES SHALL BE TERMINATED AT THE MAIN LINE PER UTILITY COMPANY STANDARDS. THE CONTRACTOR SHALL REMOVE ALL ABANDONED UTILITIES LOCATED WITHIN THE LIMITS OF WORK, UNLESS SPECIFICALLY IDENTIFIED OTHERWISE.
  - PAVEMENT REMOVAL LIMITS ARE SHOWN FOR CONTRACTOR'S CONVENIENCE. ADDITIONAL PAVEMENT REMOVAL MAY BE REQUIRED DEPENDING ON THE CONTRACTOR'S OPERATION. CONTRACTOR TO VERIFY FULL LIMITS OF PAVEMENT REMOVAL PRIOR TO BID.
  - SAW CUT AND REMOVE PAVEMENT AND CONSTRUCT PAVEMENT TRENCH PATCH FOR ALL UTILITIES TO BE REMOVED AND PROPOSED UTILITIES LOCATED IN EXISTING PAVEMENT AREAS TO REMAIN.
  - THE CONTRACTOR IS RESPONSIBLE FOR ALL NECESSARY TRAFFIC CONTROL/SAFETY DEVICES TO ENSURE SAFE VEHICULAR AND PEDESTRIAN ACCESS THROUGH THE WORK AREA, OR FOR SAFELY IMPLEMENTING DETOURS AROUND THE WORK AREA. PERFORM TRAFFIC CONTROL IN ACCORDANCE WITH THE CONTRACTOR'S APPROVED TRAFFIC CONTROL PLAN.
  - THE CONTRACTOR SHALL MAINTAIN EMERGENCY ACCESS TO ALL PROPERTIES WITHIN THE PROJECT AREA AT ALL TIMES DURING CONSTRUCTION.
  - THE CONTRACTOR SHALL PHASE DEMOLITION AND CONSTRUCTION AS REQUIRED TO PROVIDE CONTINUOUS SERVICE TO EXISTING BUSINESSES AND HOMES THROUGHOUT THE CONSTRUCTION PERIOD. EXISTING BUSINESS AND HOME SERVICES INCLUDE, BUT ARE NOT LIMITED TO ELECTRICAL, COMMUNICATION, FIRE PROTECTION, DOMESTIC WATER AND SEWER SERVICES. TEMPORARY SERVICES, IF REQUIRED, SHALL COMPLY WITH ALL FEDERAL, STATE, LOCAL AND UTILITY COMPANY STANDARDS. CONTRACTOR SHALL PROVIDE DETAILED CONSTRUCTION SCHEDULE TO OWNER PRIOR TO ANY DEMOLITION/CONSTRUCTION ACTIVITIES AND SHALL COORDINATE TEMPORARY SERVICES TO ABUTTERS WITH THE UTILITY COMPANY AND AFFECTED ABUTTER.
  - THE CONTRACTOR SHALL PROTECT ALL PROPERTY MONUMENTATION THROUGHOUT DEMOLITION AND CONSTRUCTION OPERATIONS. SHOULD ANY MONUMENTATION BE DISTURBED BY THE CONTRACTOR, THE CONTRACTOR SHALL EMPLOY A MASSACHUSETTS LICENSED SURVEYOR TO REPLACE DISTURBED MONUMENTS.
  - THE CONTRACTOR SHALL PAY ALL COSTS NECESSARY FOR TEMPORARY PARTITIONING, BARRICADING, FENCING, SECURITY AND SAFETY DEVICES REQUIRED FOR THE MAINTENANCE OF A CLEAN AND SAFE CONSTRUCTION SITE.
  - THE CONTRACTOR SHALL OBTAIN AND PAY FOR ADDITIONAL PERMITS, NOTICES AND FEES NECESSARY TO COMPLETE THE WORK AND ARRANGE FOR AND PAY FOR NECESSARY INSPECTIONS AND APPROVALS FROM THE AUTHORITIES HAVING JURISDICTION.
  - THE CONTRACTOR SHALL COORDINATE ALL WORK WITHIN PUBLIC RIGHT OF WAY WITH THE TOWN OF BARNSTABLE. ALL WORK SHALL CONFORM TO THE TOWN OF BARNSTABLE DEPARTMENT OF PUBLIC WORKS, STANDARD SPECIFICATIONS AND WITH THE COMMONWEALTH OF MASSACHUSETTS HIGHWAY DEPARTMENT "STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES," CURRENT EDITION.
  - NOTIFY THE ENGINEER IN WRITING OF ANY CONFLICT, ERROR, AMBIGUITY, OR DISCREPANCY WITH THE PLANS OR BETWEEN THE PLANS AND ANY APPLICABLE LAW, REGULATION, CODE, STANDARD SPECIFICATION, OR MANUFACTURER'S INSTRUCTIONS.
  - TIGHE & BOND ASSUMES NO RESPONSIBILITY FOR ANY ISSUES, LEGAL OR OTHERWISE, RESULTING FROM CHANGES MADE TO THESE DRAWINGS WITHOUT WRITTEN AUTHORIZATION FROM TIGHE & BOND.
  - BOLD LINETYPES AND FEATURES SHOWN WITHIN THE LIMITS OF WORK INDICATE SITE FEATURES TO BE REMOVED, WHILE FADED LINETYPES AND FEATURES ARE TO REMAIN, UNLESS SPECIFICALLY IDENTIFIED OTHERWISE.



**ADDITIONAL EXISTING CONDITIONS & DEMOLITION LEGEND**

	PROPERTY LINE
	ADJACENT PROPERTY LINE
	EASEMENT LINE
	LIMIT OF WORK (APPROX)
	LIMIT OF SAWCUT (APPROX)
	LIMIT OF EROSION CONTROL (APPROX)
	PAVEMENT / CONCRETE SECTION TO BE REMOVED (APPROX)
	LIMIT OF BUILDING TO BE REMOVED (APPROX)



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Town of  
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Barnstable,  
Massachusetts

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DESIGNED BY:	M.CURLEY	
CHECKED BY:	E.DOREMUS	
APPROVED BY:	P.CRIMMINS	

**SITE AND GRADING PLAN**

SCALE: AS SHOWN

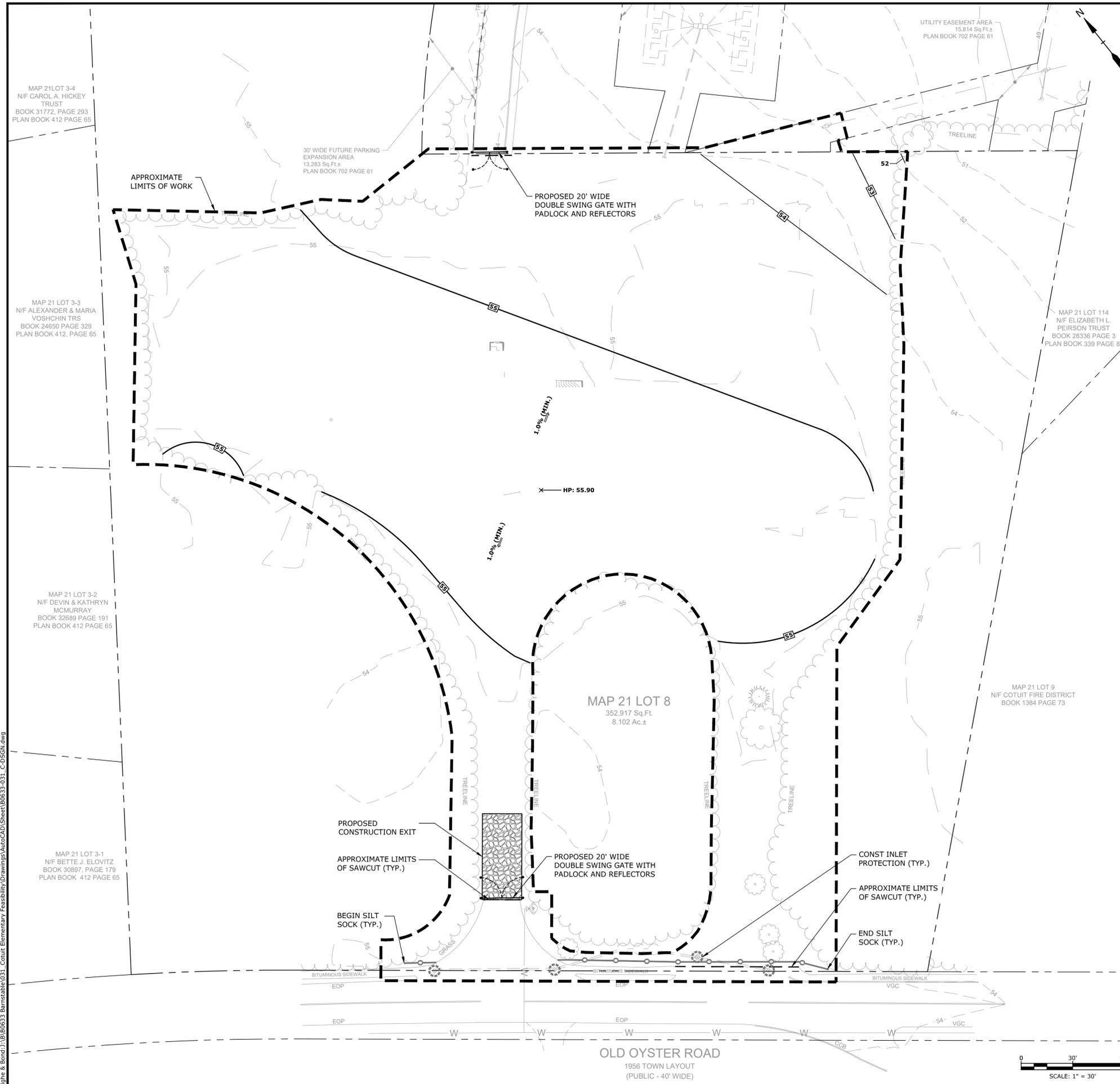
C-201

- GRADING AND DRAINAGE NOTES:**
- GENERAL COMPACTION REQUIREMENTS:
    - BELOW LOAM AND SEED AREAS: 90%
    - \* ALL PERCENTAGES OF COMPACTION SHALL BE OF THE MAXIMUM DRY DENSITY AT THE OPTIMUM MOISTURE CONTENT AS DETERMINED AND CONTROLLED IN ACCORDANCE WITH ASTM D-1557, METHOD C FIELD DENSITY TESTS SHALL BE MADE IN ACCORDANCE WITH ASTM D-1556 OR ASTM-2922.
  - THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CATCH BASINS AND DRAIN LINES, WITHIN THE LIMIT OF WORK, OF SEDIMENT IMMEDIATELY UPON COMPLETION OF CONSTRUCTION.
  - ALL MATERIALS AND CONSTRUCTION SHALL CONFORM WITH APPLICABLE FEDERAL, STATE AND LOCAL CODES.
  - ALL WORK SHALL CONFORM TO THE TOWN OF BARNSTABLE DEPARTMENT OF PUBLIC WORKS, STANDARD SPECIFICATIONS AND WITH THE COMMONWEALTH OF MASSACHUSETTS HIGHWAY DEPARTMENT "STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES," CURRENT EDITION.
  - NOTIFY THE ENGINEER IN WRITING OF ANY CONFLICT, ERROR, AMBIGUITY, OR DISCREPANCY WITH THE PLANS OR BETWEEN THE PLANS AND ANY APPLICABLE LAW, REGULATION, CODE, STANDARD SPECIFICATION, OR MANUFACTURER'S INSTRUCTIONS. TIGHE & BOND ASSUMES NO RESPONSIBILITY FOR ANY ISSUES, LEGAL OR OTHERWISE, RESULTING FROM CHANGES MADE TO THESE DRAWINGS WITHOUT WRITTEN AUTHORIZATION FROM TIGHE & BOND.

- GENERAL EROSION CONTROL NOTES:**
- ALL EROSION CONTROL MEASURES AND PRACTICES SHALL CONFORM TO THE "MASSACHUSETTS EROSION AND SEDIMENT CONTROL GUIDELINES FOR URBAN AND SUBURBAN AREAS", PREPARED BY THE MASSACHUSETTS EXECUTIVE OFFICE OF ENVIRONMENTAL AFFAIRS.
  - CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR ALL EROSION AND SEDIMENT CONTROL MEASURES FOR APPROVAL.
  - AS THE FIRST ORDER OF WORK, PRIOR TO ANY EARTH DISTURBANCE, THE CONTRACTOR SHALL INSTALL ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES INCLUDING BUT NOT LIMITED TO; INLET PROTECTION BARRIERS, SILT SOCKS, SILT FENCES, MULCH BERMS, AND STABILIZED CONSTRUCTION EXITS AS SHOWN ON THE DRAWINGS, AND AS REQUIRED BY ALL STATE AND LOCAL PERMITS AND APPROVALS.
  - INLET PROTECTION BARRIERS SHALL BE INSTALLED AT ALL EXISTING AND PROPOSED CATCH BASINS/CURB INLETS AND YARD DRAINS WITHIN THE LIMITS OF WORK AS WELL AS ANY CATCH BASINS/CURB INLETS AND YARD DRAINS THAT RECEIVE RUNOFF FROM ANY CONSTRUCTION ACTIVITIES. THESE MEASURES SHALL BE FULLY MAINTAINED FOR THE DURATION OF THE PROJECT.
  - PERIMETER CONTROLS INCLUDING SILT FENCES, MULCH BERM, SILT SOCK, AND/OR HAY BALE BARRIERS SHALL BE MAINTAINED FOR THE DURATION OF THE PROJECT UNTIL ALL NON-PAVED AREAS HAVE BEEN STABILIZED.
  - THE CONTRACTOR SHALL INSTALL EROSION CONTROL BLANKETS ON ALL STEEP SLOPE AREA (3:1 OR GREATER).
  - DURING CONSTRUCTION THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTROLLING DUST AND WIND EROSION THROUGHOUT THE CONSTRUCTION PERIOD. DUST CONTROL MEASURES INCLUDE, BUT NOT LIMITED TO; MOISTEN EXPOSED SOIL SURFACES PERIODICALLY WITH ADEQUATE WATER TO CONTROL DUST, MECHANICAL SWEEPERS ON PAVED SURFACES, AND COVERING SURFACES WITH CRUSHED STONE OR COARSE GRAVEL.
  - THE CONTRACTOR SHALL HAVE ALL EROSION AND SEDIMENT CONTROL MEASURES INCLUDING, BUT NOT LIMITED TO, INLET PROTECTION BARRIERS, PERIMETER SEDIMENT CONTROLS, AND STEEP SLOPE EROSION CONTROL BLANKETS INSPECTED BY A QUALIFIED PERSON AT LEAST ONCE A WEEK AND WITHIN 24 HOURS AFTER A RAIN EVENT OF 0.25 INCHES OR GREATER. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE ADJUSTED, REPAIRED, MODIFIED, OR ADDITIONAL MEASURES INSTALLED AS NECESSARY TO ADDRESS EVOLVING CONDITIONS DURING CONSTRUCTION.
  - SEDIMENT CONTROL FILTER MEASURES SHALL BE REPLACED WHEN SEDIMENT REACHES 1/3 THE HEIGHT OF THE FILTER.
  - THE CONTRACTOR SHALL REMOVE AND PROPERLY DISPOSE OF ALL TEMPORARY EROSION CONTROL DEVICES UPON COMPLETION OF CONSTRUCTION.
  - ALL CATCH BASIN SUMPS AND PIPING SHALL BE THOROUGHLY CLEANED TO REMOVE ALL SEDIMENT AND DEBRIS AFTER THE PROJECT HAS BEEN FULLY STABILIZED.
  - TEMPORARY SOIL STOCKPILE SHALL BE SURROUNDED WITH SEDIMENT CONTROLS AND SHALL BE STABILIZED BY TEMPORARY EROSION CONTROL SEEDING. STOCKPILE AREAS TO BE LOCATED AS FAR AS POSSIBLE FROM THE DELINEATED EDGE OF WETLANDS. STOCKPILES OVER 10 FEET SHALL HAVE SAFETY FENCING PROVIDED AROUND THE STOCKPILES.
  - IF NECESSARY TEMPORARY SEDIMENT TRAPS SHALL BE PROVIDED FOR GENERAL EXCAVATION DEWATERING PRACTICES PRIOR TO DIRECTING FLOW TO ANY OTHER EROSION AND SEDIMENT CONTROL MEASURES.
  - THE CONTRACTOR SHALL BE FAMILIAR WITH SPILL PREVENTION MEASURES REQUIRED BY ALL LOCAL, STATE, AND FEDERAL REGULATIONS.

**PROPOSED GRADING, DRAINAGE, & EROSION CONTROL LEGEND**

- INTERMEDIATE CONTOURS
- INDEX CONTOURS
- SPOT / APPROX SPOT GRADE
- MAGNITUDE & DIRECTION OF SLOPE
- LIMIT OF EROSION CONTROL (APPROX)
- INLET PROTECTION



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**PLANTING MATERIALS LEGEND**

SYM.	DET.	DESCRIPTION
(P1)		EXISTING TREE TO REMAIN
(P2)	(1 L-201)	DECIDUOUS TREE
(P3)	(1 L-201)	EVERGREEN TREE
(P4)	(3 L-201)	EROSION CONTROL/RESTORATION SEED MIX
(P5)	(3 L-201)	LOGGING ROAD SEED MIX
(P6)	(2 L-201)	SHRUB PLANTING - REFER TO PLANTING SCHEDULE FOR SPECIES AND QUANTITY

**PLANTING NOTES**

- LANDSCAPE ARCHITECT TO APPROVE PLANT MATERIAL PRIOR TO DELIVERY TO SITE.
- PLANT MATERIAL SHALL CONFORM TO "THE AMERICAN STANDARD FOR NURSERY STOCK", PUBLISHED BY AMERICAN HORT.
- NO SUBSTITUTIONS OF PLANT SPECIES WITHOUT LANDSCAPE ARCHITECT'S WRITTEN APPROVAL.
- SUBSTITUTIONS OF PLANT SPECIES SHALL BE A PLANT OF EQUIVALENT OVERALL FORM, HEIGHT AND BRANCHING HABIT, FLOWER, LEAF AND FRUIT, COLOR AND TIME OF BLOOM, AS APPROVED BY LANDSCAPE ARCHITECT.
- LOCATE AND VERIFY UTILITY LINE LOCATIONS PRIOR TO STAKING AND REPORT CONFLICTS TO LANDSCAPE ARCHITECT.
- PLANTING DEMOLITION DEBRIS, GARBAGE, LUMPS OF CONCRETE, STEEL AND OTHER MATERIALS DELETERIOUS TO PLANT'S HEALTH AS DETERMINED BY LANDSCAPE ARCHITECT SHALL BE REMOVED FROM ALL PLANTING AREAS.
- NO PLANTING TO BE INSTALLED BEFORE ACCEPTANCE OF ROUGH GRADING.
- ALL PROPOSED TREE LOCATIONS SHALL BE STAKED OR LAID OUT IN THEIR APPROXIMATE LOCATION BY THE CONTRACTOR. REFER TO LAYOUT AND PLANTING SHEETS FOR LAYOUT INFORMATION. THE CONTRACTOR SHALL ADJUST THE LOCATIONS AS REQUESTED BY THE LANDSCAPE ARCHITECT TO ACCOUNT FOR SUBSURFACE UTILITIES AND OTHER FIELD CONDITIONS. FINAL LOCATIONS OF ALL PLANTS MUST BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO PLANTING.
- INSTALL PLANTS WITH ROOT FLARES FLUSH WITH FINISHED GRADE. IMMEDIATELY REPLANT PLANTS THAT SETTLE OUT OF PLUMB OR BELOW FINISHED GRADE.
- PLANT UNDER FULL TIME SUPERVISION OF CERTIFIED ARBORIST, NURSERYMAN, OR LICENSED LANDSCAPE ARCHITECT. PROVIDE WRITTEN VERIFICATION OF CERTIFICATION AND/OR LICENSE FOR LANDSCAPE ARCHITECT'S APPROVAL.
- WATER PLANTS THOROUGHLY AFTER INSTALLATION, A MINIMUM OF TWICE WITHIN THE FIRST 24 HOURS.
- REPAIR DAMAGE DUE TO OPERATIONS INSIDE AND OUTSIDE OF LIMIT OF WORK
- SOAK ALL PERENNIALS FOR 24 HOURS PRIOR TO INSTALLATION
- SEE CIVIL ENGINEER DRAWINGS FOR GRADING INFORMATION

PLANT SCHEDULE				
Symbol	Quantity	Botanical Name	Common Name	Size
<b>TREES</b>				
AC RU	8	<i>Acer Rubrum 'October Glory'</i>	Red Maple	2-2.5" caliper, B&B
LI TU	8	<i>Liriodendron tulipifera</i>	Tulip Tree	2-2.5" caliper, B&B
PI RI	16	<i>Pinus rigida</i>	Pitch Pine	10'-12' Ht.
PI ST	8	<i>Pinus strobus</i>	White Pine	10'-12' Ht.
QU AL	10	<i>Quercus alba</i>	White Oak	2-2.5" caliper, B&B
QU RU	10	<i>Quercus rubra</i>	Northern Red Oak	2-2.5" caliper, B&B
<b>SHRUBS</b>				
	136	<i>Comptonia peregrina</i>	Sweet Fern	3 gal. Pot
	136	<i>Cornus sericea</i>	Red Twig Dogwood	30-36" ht
	340	<i>Fothergilla Gardenii</i>	Dwarf Witch Alder	24-30" ht
	136	<i>Ilex Verticillata</i>	Winterberry	24-30" ht
	204	<i>Morella carolinensis</i>	Bayberry	3 gal. Pot
	136	<i>Rhus aromatica 'Low Grow'</i>	Low Grow Sumac	3 gal. Pot
	272	<i>Viburnum Dentatum</i>	Arrowwood	30-36" ht
<b>SEED MIXES</b>				
		New England Logging Road Mix	New England Wetland Plants	
		New England Erosion Control/Restoration Mix for Dry Sites	New England Wetland Plants	

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Barnstable, Massachusetts

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PROJECT NO:	B0633-031	
DATE:	02/17/2026	
FILE:	B0633-031_L-DSGN.dwg	
DRAWN BY:	LB	
DESIGNED BY:	X.XXXXX	
CHECKED BY:	JF	
APPROVED BY:	JF	

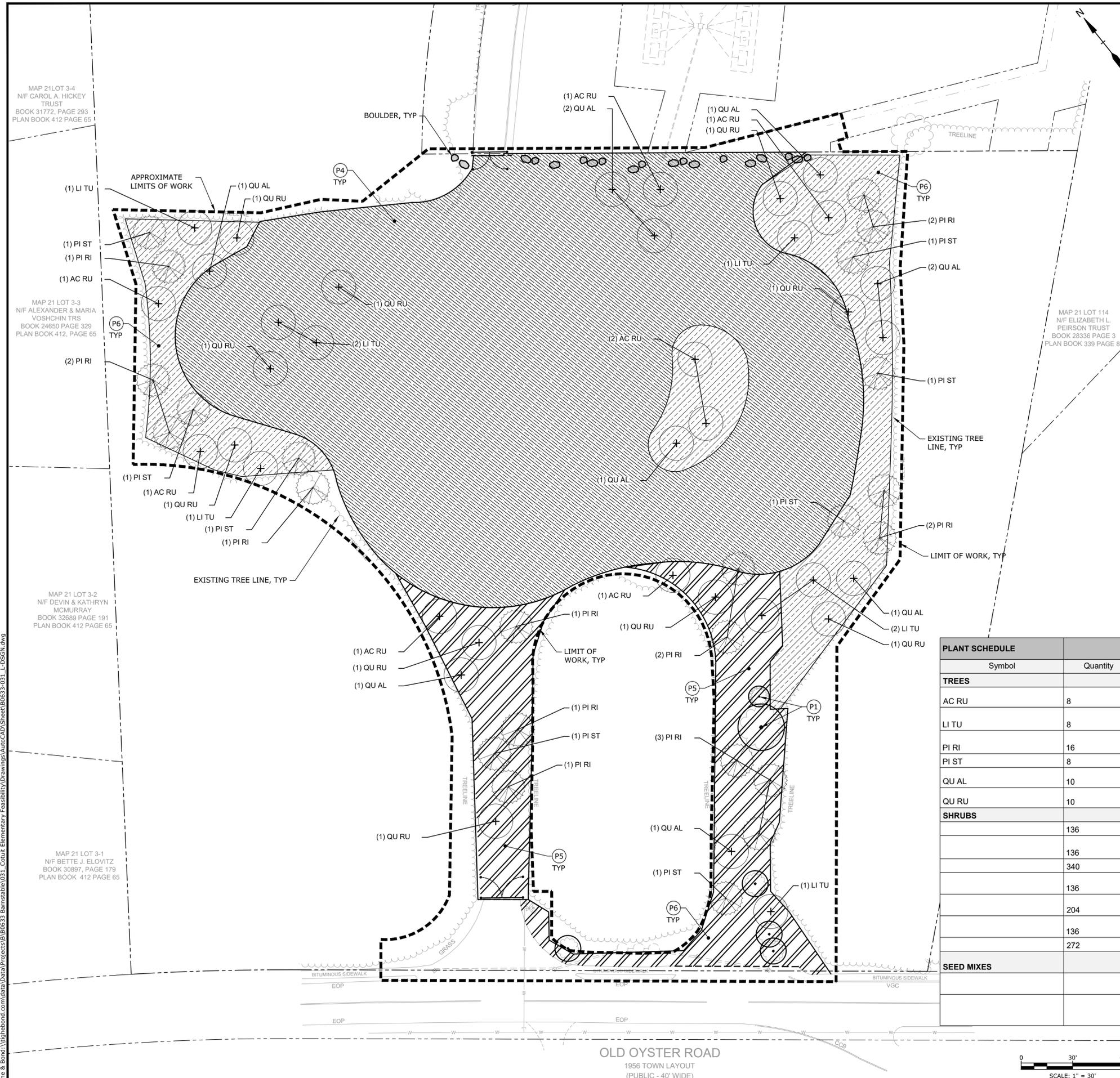
LANDSCAPE PLANTING PLAN

SCALE: AS SHOWN

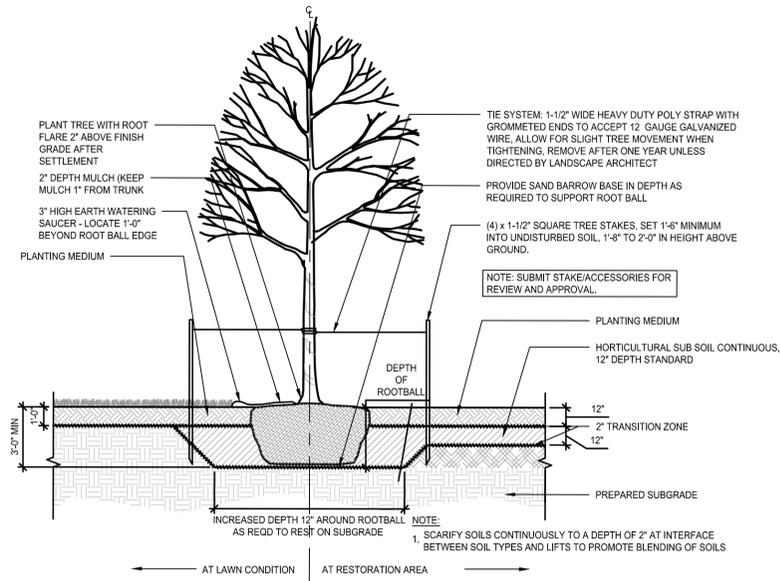
L-101

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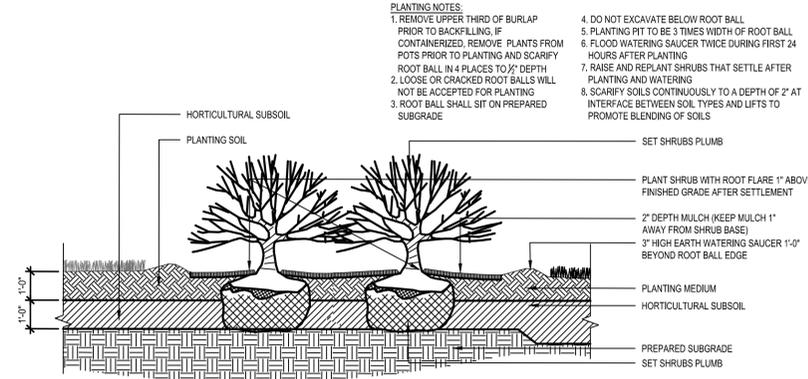
OLD OYSTER ROAD  
1956 TOWN LAYOUT  
(PUBLIC - 40' WIDE)



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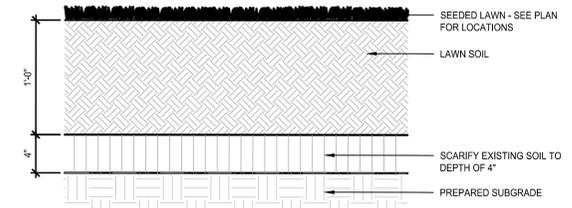


**1 TREE PLANTING - IN LAWN OR RESTORATION AREAS**  
Scale: 1/4"=1'-0"



**2 SHRUB PLANTING**  
Scale: 3/8"=1'-0"

**PLANTING NOTES:**  
1. REMOVE UPPER THIRD OF BURLAP PRIOR TO BACKFILLING. IF CONTAINERIZED, REMOVE PLANTS FROM POTS PRIOR TO PLANTING AND SCARIFY ROOT BALL IN 4 PLACES TO 2" DEPTH  
2. LOOSE OR CRACKED ROOT BALLS WILL NOT BE ACCEPTED FOR PLANTING  
3. ROOT BALL SHALL SIT ON PREPARED SUBGRADE  
4. DO NOT EXCAVATE BELOW ROOT BALL  
5. PLANTING PIT TO BE 3 TIMES WIDTH OF ROOT BALL  
6. FLOOD WATERING SAUCER TWICE DURING FIRST 24 HOURS AFTER PLANTING  
7. RAISE AND REPLANT SHRUBS THAT SETTLE AFTER PLANTING AND WATERING  
8. SCARIFY SOILS CONTINUOUSLY TO A DEPTH OF 2" AT INTERFACE BETWEEN SOIL TYPES AND LIFTS TO PROMOTE BLENDING OF SOILS



**3 SEEDED LAWN**  
Scale: 1-1/2"=1'-0"

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LANDSCAPE DETAILS

SCALE: AS SHOWN

L-201

**DRAFT**

# COTUIT ELEMENTARY SCHOOL DEMOLITION

140 OLD OYSTER ROAD  
COTUIT, MA

ARCHITECT



CONSULTANT

STAMP

KEY PLAN

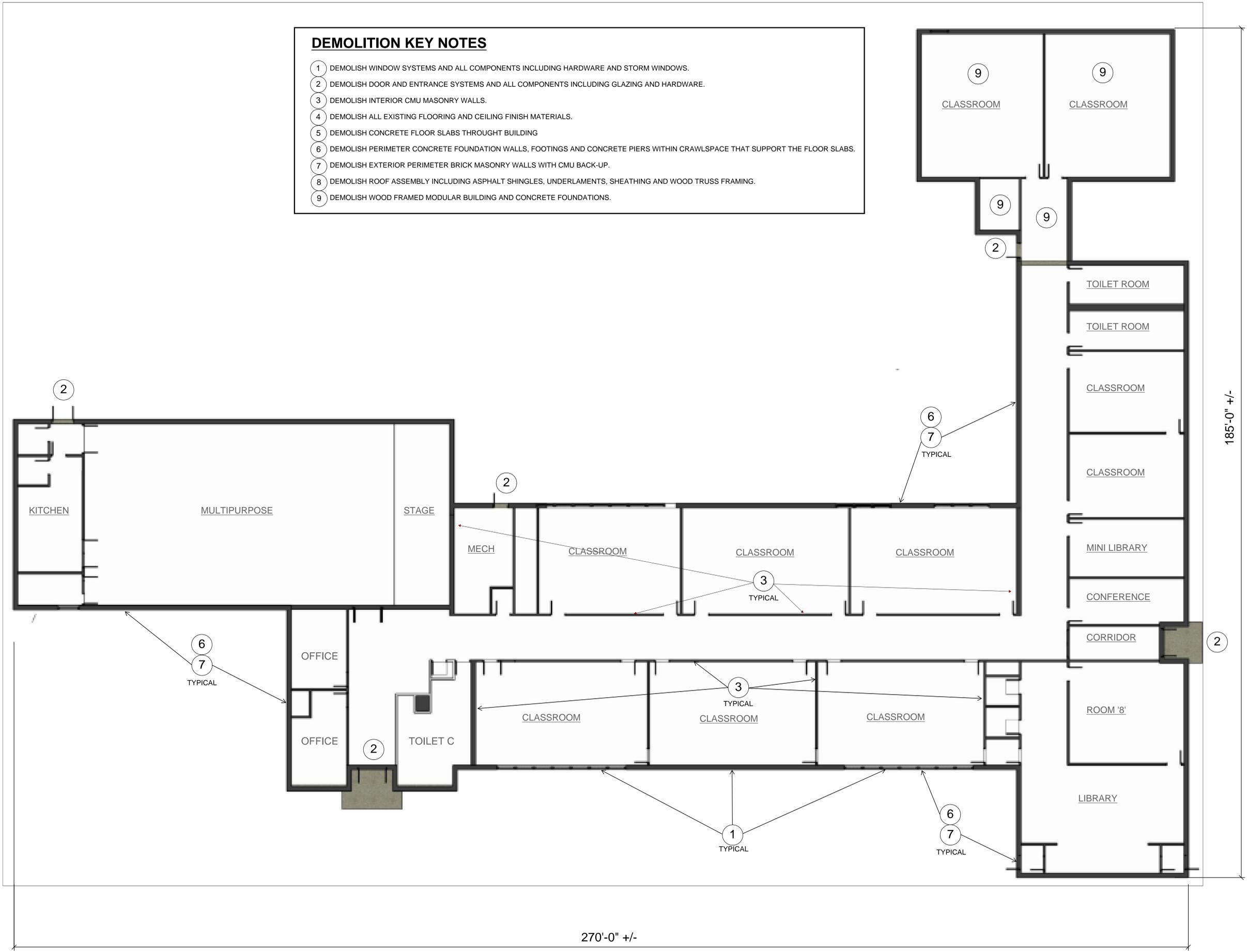
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PROJECT NUMBER: 225005.00		
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CHECKED BY: Checker		

SHEET TITLE

DEMOLITION PLAN

A-101

- ### DEMOLITION KEY NOTES
- 1 DEMOLISH WINDOW SYSTEMS AND ALL COMPONENTS INCLUDING HARDWARE AND STORM WINDOWS.
  - 2 DEMOLISH DOOR AND ENTRANCE SYSTEMS AND ALL COMPONENTS INCLUDING GLAZING AND HARDWARE.
  - 3 DEMOLISH INTERIOR CMU MASONRY WALLS.
  - 4 DEMOLISH ALL EXISTING FLOORING AND CEILING FINISH MATERIALS.
  - 5 DEMOLISH CONCRETE FLOOR SLABS THROUGHOUT BUILDING
  - 6 DEMOLISH PERIMETER CONCRETE FOUNDATION WALLS, FOOTINGS AND CONCRETE PIERS WITHIN CRAWLSPACE THAT SUPPORT THE FLOOR SLABS.
  - 7 DEMOLISH EXTERIOR PERIMETER BRICK MASONRY WALLS WITH CMU BACK-UP.
  - 8 DEMOLISH ROOF ASSEMBLY INCLUDING ASPHALT SHINGLES, UNDERLAMENTS, SHEATHING AND WOOD TRUSS FRAMING.
  - 9 DEMOLISH WOOD FRAMED MODULAR BUILDING AND CONCRETE FOUNDATIONS.

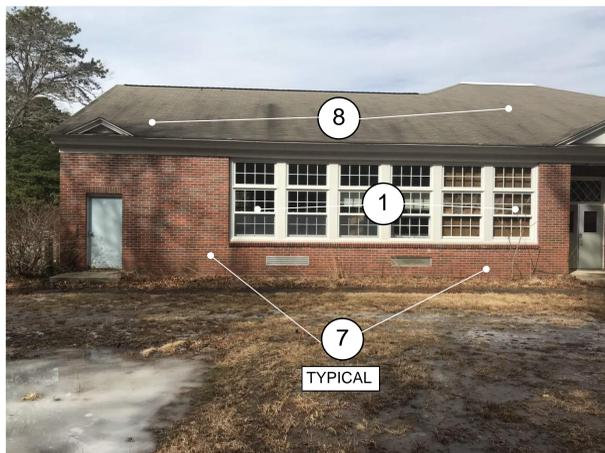


1 DEMOLITION PLAN

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1 ELEVATION 1



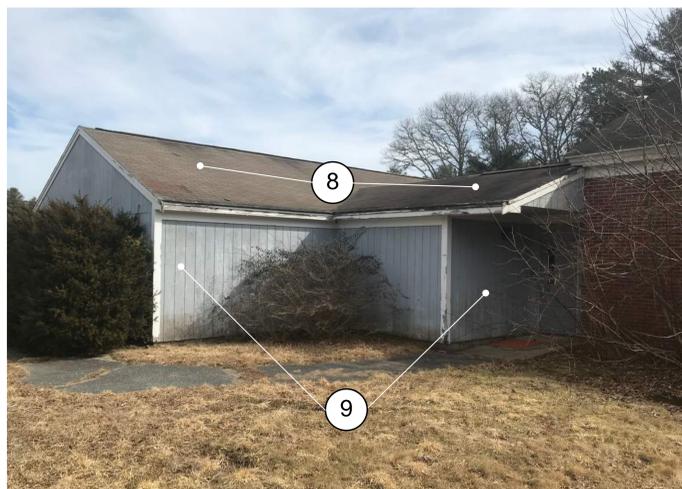
2 ELEVATION 2



3 ELEVATION 3



4 ELEVATION 4



5 ELEVATION 5



6 ELEVATION 6

COTUIT  
ELEMENTARY  
SCHOOL  
DEMOLITION

140 OLD OYSTER ROAD  
COTUIT, MA

ARCHITECT

**E-ICON**  
ARCHITECTURE  
101 SUMMER ST BOSTON MA 02110

CONSULTANT

STAMP

KEY PLAN

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DEMOLITION  
PHOTOGRAPHS

A-102



1 TYP. CLASSROOM



2 TYP. CORRIDOR



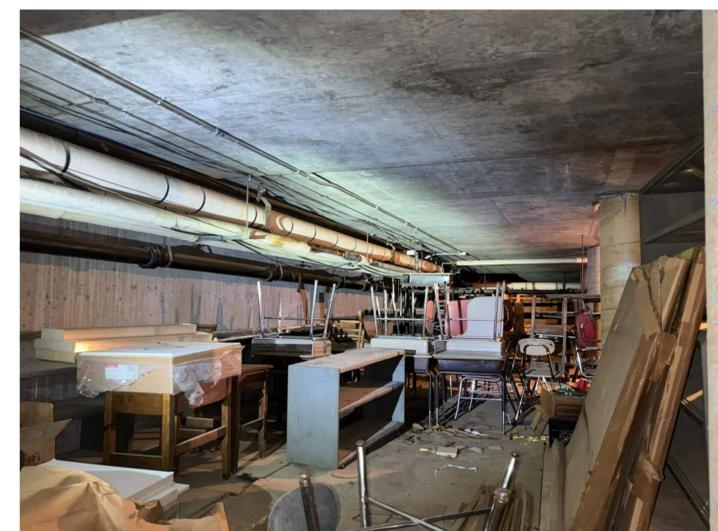
3 MULTI PURPOSE SPACE



4 KITCHEN



5 BASEMENT



6 BASEMENT



7 CRAWL SPACE



8 CRAWL SPACE



7 ATTIC SPACE

# COTUIT ELEMENTARY SCHOOL DEMOLITION

140 OLD OYSTER ROAD  
COTUIT, MA

ARCHITECT

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